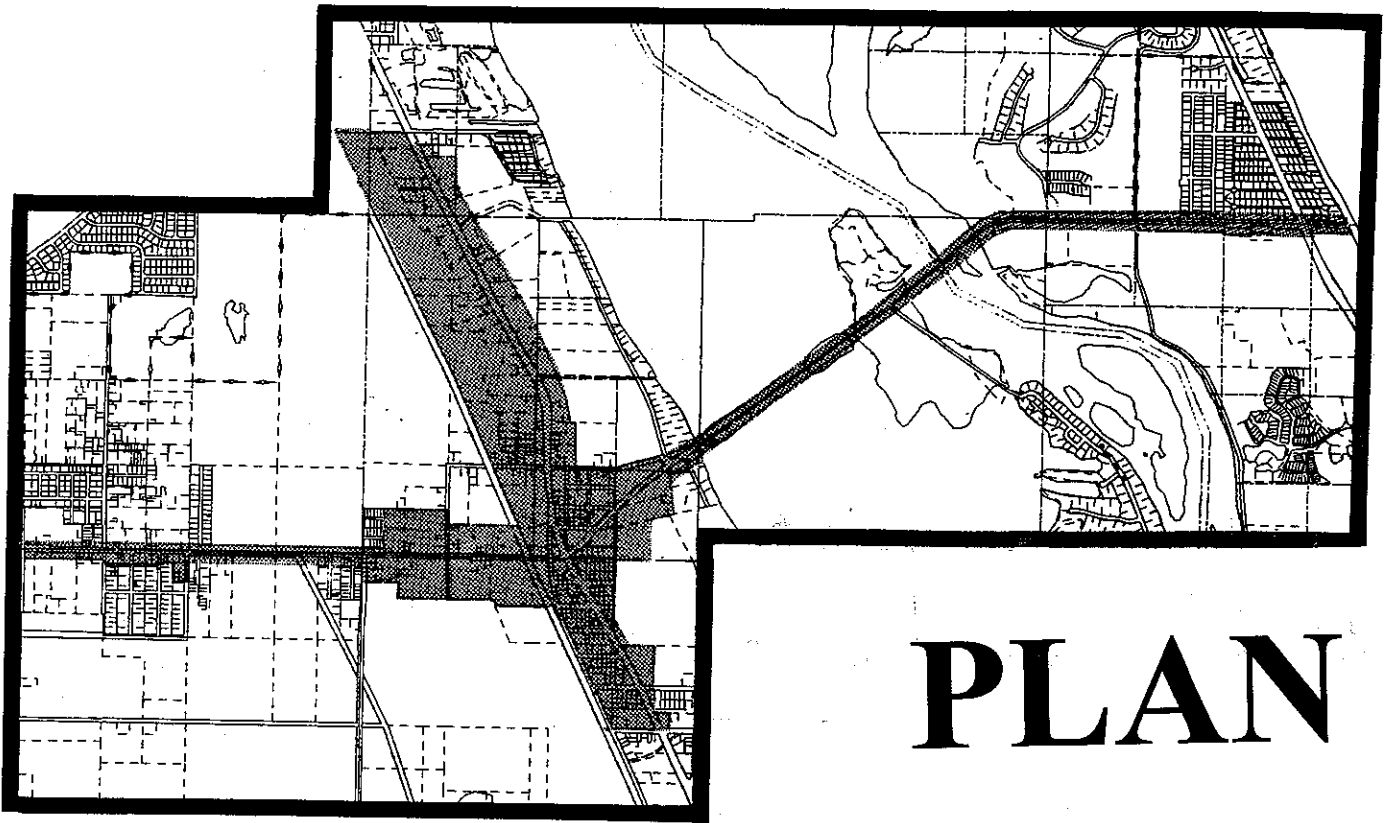

WABASSO CORRIDOR



PLAN
2000

WABASSO CORRIDOR PLAN

PREPARED BY: WABASSO CORRIDOR PLAN TASK FORCE
IN COOPERATION WITH THE
INDIAN RIVER COUNTY
COMMUNITY DEVELOPMENT DEPARTMENT
AND
URBAN RESOURCE GROUP

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BACKGROUND

INTRODUCTION

Within Indian River County, there are both incorporated municipalities and unincorporated communities. While the unincorporated settlements lack the formal government structure and local control of cities and towns, each of the county's unincorporated communities nevertheless has a unique character and identity. One of those unincorporated communities is Wabasso.

As one of the oldest communities in the county, Wabasso is historically significant. Besides being historically significant, Wabasso is centrally located, proximate to the Indian River Lagoon, and situated at the confluence of north/south, east/west, and mainland/island traffic routes. All of these characteristics, as well as recent development trends, indicate that Wabasso may experience substantial growth in the near future.

Faced with the prospect of increasing growth and development, the Wabasso community has taken the initiative to control development in the Wabasso area. Realizing that development and redevelopment occurring within the Wabasso area in the near future will affect the Wabasso community for generations, the residents, property owners, and business owners in Wabasso have prepared this corridor plan.

This corridor plan is a recognition that the county's current land development regulations, while generally adequate, are insufficient to ensure the quality of development desired by some communities. Consequently, this corridor plan incorporates standards applicable for future development in the Wabasso corridor. As such, the corridor plan represents a balance of interests and a plan for future development consistent with the community's desire.

PURPOSE AND VISION

The overall purpose of the Wabasso Corridor Plan is to provide a set of standards applicable to development and redevelopment in the corridor area. Specifically, the plan's purpose is:

- To preserve and enhance the appearance of the Wabasso Corridor

- To recognize and enhance the character of the Wabasso community
- To increase property values in the Wabasso corridor
- To prevent the establishment of incompatible land uses and unattractive developments in the corridor
- To coordinate public and private investments to enhance the function and appearance of the corridor

These statements of purpose are derived from the community's vision for Wabasso. As a brief description of how the community would like Wabasso to look in the future, the vision statement represents a consensus among Wabasso's residents, business owners, and property owners. The community's vision is as follows:

WABASSO WILL RETAIN THE FEELING OF AN UNCLUTTERED, WELL MAINTAINED SMALL TOWN. THROUGH ATTRACTIVE LANDSCAPING AND SPECIAL DEVELOPMENT DESIGN CONSIDERATIONS, WABASSO WILL PRESERVE AND ENHANCE THE NATURAL BEAUTY AND SCENIC VISTAS THAT GIVE WABASSO ITS SPECIAL CHARACTER. ADDITIONALLY, THIS AREA WILL BE CHARACTERIZED BY SAFE UNCONGESTED ROADS, AND BY A MIX OF USES INCLUDING LARGE AND SMALL

PLAN DEVELOPMENT PROCESS

Unlike many planning initiatives, the Wabasso Corridor planning process was community initiated. Instead of being mandated by the county or the state, the Wabasso Corridor planning effort came about as a result of a request by several members of the Wabasso community.

Recognizing that growth pressures were increasing in Wabasso, several Wabasso property owners approached planning staff in July, 1994 and requested assistance in establishing requirements to ensure that future growth in the Wabasso Corridor would enhance the area's appearance and scenic qualities. Together, planning staff

and Wabasso property owners approached the Board of County Commissioners with this request. In August, 1994, the Board directed planning staff to work with members of the Wabasso community and prepare a Wabasso Corridor Plan.

Assisted by Urban Resource Group, a Vero Beach land planning and landscape architecture firm, planning staff did research, compiled information, prepared background material, and held three community meetings in an effort to prepare a corridor plan. While the first two meetings were successful in identifying community concerns and community desires, the third meeting was unsuccessful. At that meeting, a small group of people opposed to the planning effort derailed the meeting and effectively stalled the planning process.

Undeterred, a core group of Wabasso landowners circulated petitions within the community and demonstrated overwhelming support for continuation of the corridor planning process. In response to this indication of support, the Board of County Commissioners established a Wabasso Corridor Plan Task Force to work with planning staff to develop a corridor plan.

Consisting of fifteen members, the task force met eight times from May, 1995 through September, 1995, including a field trip to the City of Port Orange. Through this process, the task force reviewed background data and information, developed objectives, and considered regulatory standards. Through compromise, consensus building, and a balance of interests, the task force prepared and adopted this corridor report.

CORRIDOR AREA DESCRIPTIONS AND CONDITIONS

Historically, Wabasso has been a semi-rural area dominated by the citrus industry. Several of the oldest homes in the county are located in Wabasso.

CORRIDOR BOUNDARIES

Being an unincorporated area, Wabasso has no legal boundaries. The corridor's boundaries, therefore, were established to include the area generally known as Wabasso. The boundaries run along US 1 from 77th Street (all sides of the 77th Street/US 1 intersection) to 95th Street and along CR 510 from 66th Avenue (all sides of 66th Avenue/CR 510 intersection) to the Atlantic Ocean. The planning area boundaries are shown on figures 1, 2, and 3.

The heart of Wabasso is located at the intersection of the area's two main roads--CR 510 and US 1. East of 58th Avenue, most properties within the planning area that front these roads have a commercial/industrial land use designation (see figure 4). The eastern portion of the planning area, as well as the portion west of 58th Avenue, are mainly designated for residential development (see figures 4 and 5).

CR 510 and US 1 are both classified as urban principal arterials on the county's future roadway thoroughfare plan map. US 1 is the main north-south roadway through the county, while CR 510 is one of the major east-west roadways in the county. CR 510 connects the barrier island to I-95 and western portions of the county.

- Existing and Planned Land Use and Zoning

The US 1 Corridor currently contains a mix of commercial uses, single- and multiple-family residential uses, citrus groves and vacant land. Land along CR 510 between 66th Avenue and 58th Avenue contains mostly residential uses. The exception is a 5.28 acre commercial area on the south side of CR 510. That node contains a motel, two bars, and a pool hall. Between the Florida East Coast Railroad Tracks and 58th Avenue, land along CR 510 is presently characterized by heavy commercial and light industrial development. Between 46th Avenue and SR A1A, residential land uses dominate. This mixture of uses is an important characteristic of the Wabasso area, and it is reflected in the area's zoning pattern. Figures 4 and 5 show the existing land uses in the planning area. Zoning districts within the planning area are shown on Figures 8 and 9.

- Existing and Planned Roadway Conditions and Improvements

CR 510 is a 2-lane arterial roadway with approximately 80 feet of existing public road right-of-way. According to the county's comprehensive plan, CR 510 is planned to expand to 6 lanes and 120 feet of public road right-of-way. By Fiscal Year 1998-99, an eight foot wide bikepath is programmed to be installed along CR 510 from 66th Avenue to SR A1A.

US 1 is a 4-lane arterial roadway with approximately 106 feet of existing public road right-of-way and is planned for expansion to 6 lanes and 120 feet of public road right-of-way. To the west of US 1 is Old Dixie Highway a 2-lane collector roadway with approximately 70 feet of existing public road right-of-way. Currently, no expansion of Old Dixie Highway is planned.

With 100 feet of existing right-of-way, the Florida East Coast (FEC) Railroad Tracks are adjacent to the west side of Old Dixie Highway. Figure 16 shows the functional classification of roads in the planning area, while Figure 17 shows existing and planned sidewalks and bikepaths.

- Parcel Ownership, and Size/Shape Characteristics

In Wabasso, much of US 1 was built over existing platted subdivisions. Consequently, many lots in Wabasso were affected when the state acquired right-of-way for US 1. As a result, many of these lots are oddly shaped and lack depth. These non-conforming parcels will be difficult to develop without combining parcels. At this time, approximately half of the non-conforming lots remain uncombined.

In contrast, parcels in the western portion of the planning area, along CR 510 are generally both wide and deep. Figures 2 and 3 show the individual parcels in the planning area.

Parcel ownership in Wabasso indicates that any future development within the area will be a mixture of uses. Because of these ownership characteristics, the corridor's US 1 appearance will be different than the corridor's CR 510 appearance.

- Development Potential

As shown in Figure 12, most existing development in the planning area is near the main intersection of US 1 and CR 510. Future development in that area will generally be redevelopment. Out from the main intersection, there is ample vacant land to support new development.

Several new projects in and near Wabasso are in various stages of development. The Community feels that this mini development "boom" will usher in an extended growth spurt for Wabasso. Figure 18 gives the location of the following projects.

- Disney's Florida Beach Resort

Located at the intersection of CR 510 and SR A1A, this project is a 500+ unit timeshare resort for Disney Club members. This project is Disney's first resort not located at a theme park.

- Orchid Place

This is a proposed 150 unit single-family subdivision located on the north side of CR 510, on the barrier island.

- Quick Mart

This proposed project includes a gas station and a convenience store at the northeast corner of CR 510 and US 1.

- K&R Groves Rezoning and Comprehensive Plan Land Use Change

In April 1994, K&R Groves, Inc. was granted a request to redesignate approximately 8.4 acres located at the southeast corner of CR 510 and 46th Avenue from residential to commercial. In January 1995, K&R Groves requested that the commercial designation of that tract be expanded to the south by 5.5 acres. In June 1995, the Board of County Commissioners voted to transmit that request to the State Department of Community Affairs for formal review.

ISSUES AND CONCERNS

The current development environment suggests that the pace of growth in Wabasso may soon substantially increase. Potential

problems associated with growth can include loss of open space, congested roads, deteriorating public infrastructure, visual clutter, deteriorating aesthetics, and loss of community character.

In the past decade in Florida, several areas that were unprepared for growth experienced rapid unplanned development characterized by vast swaths of concrete, lack of vegetation, poorly maintained infrastructure and buildings, visual clutter, and unattractive appearance. US 192 in Kissimmee is an example.

With that in mind, participants at the various Wabasso Corridor Workshops identified various concerns, including the following:

- Drainage. Parts of the area are subject to flooding during heavy rains. Stormwater run-off from area road widening projects may require extra right-of-way acquisition unless new development sites are designed to accommodate some of the run-off from such future road widening projects.
- Utilities. Most of Wabasso is not presently served by centralized water or wastewater lines. The area is characterized by unsightly overhead power lines.
- Code Enforcement. The presence of several dilapidated buildings indicates that current code enforcement in Wabasso has not been effective.
- Pedestrian Systems. Currently, sidewalks exist along both sides of most of US 1. In contrast, there are no sidewalks along CR 510, Old Dixie Highway, or most other roads in Wabasso.
- Vehicular Control. Because CR 510 is a 2-lane road with no turning lanes, its intersections with US 1 and with Old Dixie Highway experience backlogs and can be dangerous.
- Grandfathered Uses/Non-Conformities/Redevelopment. Much of the existing development in Wabasso was built prior to the enactment of existing setback, parking, land use, building, stormwater retention, and landscaping regulations. Additionally, several circumstances have caused Wabasso to have an inordinate number of lots that lack adequate size. For these reasons, there is much confusion among residents,

property owners, and business owners regarding which uses are permitted and when buildings must meet existing regulations.

- Building Setbacks. Many buildings and parking lots in Wabasso are located close to the road right-of-way. That situation limits the amount of planting that can occur and generally results in an unattractive overabundance of pavement.
- Signs. Most signs in Wabasso are mounted on one or two poles up to 35 feet high. Signs may be within 5 feet of the public road right-of-way.
- Architecture. Indian River County does not enforce any architectural theme regulations. Currently, buildings in Wabasso represent a mix of architectural styles.
- Landscaping. Most developed property in Wabasso meets only the minimum county landscaping requirement of 1 tree for every 30 feet of road frontage and some shrubbery.
- Rights-Of-Way. Medians are unsightly, unplanted and poorly maintained. Public works staff stated that there are some right-of-way width and acquisition constraints that will need to be addressed to accommodate future road widening projects.
- Land Uses. Vehicle sales, flea markets, outdoor storage, and industrial uses are presently permitted uses within much of Wabasso. For this reason, the potential for compatibility problems, especially with the nearby residential areas is high.
- Lighting. Roads in Wabasso have little or no lighting.
- Maintenance. Several buildings are dilapidated or poorly maintained. These buildings are characterized by unkempt vegetation, broken windows, and other signs of deterioration.
- Walls/Fences/Berms (Screening). Other than chain link fences, which are often considered unattractive, there is little screening of development from adjacent roadways.

- Lot Size. Many lots in Wabasso do not meet minimum lot size requirements. A large number of these non-conforming lots front on US 1.

OPPORTUNITIES

Despite the concerns expressed in various workshops, there is a great opportunity to favorably effect future development in the corridor. Because extensive development has not occurred in Wabasso, the opportunity exists to make effective changes and enhance the entire area. This will involve both government and private landowners.

- **Public Sector Opportunities**

Public infrastructure can have a positive or a negative effect on the appearance and desirability of an area. For that reason, government has the opportunity to improve an area as infrastructure is installed, expanded or replaced.

For this plan, infrastructure refers to roadways, sidewalks, bikepaths, and publicly maintained drainage systems. In Wabasso, infrastructure is the responsibility of county and state government. With respect to roadways, the state is responsible for US 1, while the county is responsible for CR 510.

The public sector has an opportunity to enhance an area through both the function and appearance of infrastructure. Public services that provide an adequate level of service increase the desirability of an area. Rights-of-way that are planted and well maintained enhance the appearance of an area.

In Wabasso, the CR 510 Causeway Park is an important public amenity in the corridor. A well maintained park with adequate facilities such as parking and picnic tables will complement other improvements in the corridor. Similarly, commercial activities in the park can affect the corridor.

- **Private Sector Opportunities**

Figures 4 and 5 show that there is a significant amount of vacant or agricultural land within the corridor. Additionally, much of the existing development in the area is older and soon will be in

need of redevelopment. These circumstances provide an opportunity to ensure that both new development and redevelopment in the planning area are attractive and of high quality. Accordingly, there is an opportunity to ensure that new development and redevelopment is well designed, buffered, and landscaped.

- Approaches: Regulatory Versus Non-Regulatory

A corridor plan can be implemented through a regulatory approach, a non-regulatory approach, or a combination of the two. The regulatory approach involves the adoption of specific land development regulations and the threat of sanctions for non-compliance. The non-regulatory approach relies on voluntary compliance with no sanctions.

While these approaches are different, they are not mutually exclusive. A successful plan can regulate some aspects of development, while providing only guidance with respect to other factors. Generally, the appropriate approach depends on the circumstances.

PLAN OBJECTIVES

Several components constitute the substance of a comprehensive corridor plan. These consist of the plan's objectives, proposed public improvements, regulatory standards, and voluntary guidelines. Together, these plan elements address the two most important questions inherent in any plan. These are: What does the community want and how will this be achieved?

The plan's objectives address the first question referenced above. They identify what the community wants. As such, these objectives are consistent with and derived from the community's vision for the Wabasso Corridor. The Wabasso Corridor objectives are as follows:

- Ensure that land uses along the corridor preserve the attractiveness of the corridor
- Provide public improvements that enhance the function and appearance of the corridor
- Use landscaping to buffer uses, complement building design, and enhance attractiveness of the corridor

- Minimize the clutter and unattractiveness associated with excessive signage
- Encourage architecture and design that enhances the appearance of the corridor

PUBLIC SECTOR ACTIVITIES

For the Wabasso Corridor Plan to be successful, Indian River County must participate in corridor development. Accordingly, the county will take the following actions within the corridor area:

- install utilities underground, whenever possible;
- install sidewalks in conjunction with county road and building projects;
- install sidewalks and bikepaths as programmed in the county's bikepath and sidewalk plan;
- consider abandoning Old Dixie Highway, between CR 510 and US 1;
- encourage the Florida Department of Transportation (FDOT) to enhance (landscape) US 1 medians;
- limit activities and uses within Causeway Park to those uses related to public access to the river and public events;
- enhance perimeter landscaping along Causeway Park;
- create and landscape center medians on CR 510 when right-of-way becomes available;
- improve CR 510's intersections with both US 1 and Old Dixie Highway;
- encourage new developments to be designed to accommodate some of the anticipated stormwater run-off from future road widening projects;
- adhere to corridor plan standards whenever it (the county) develops property within the corridor;

- revise the ultimate right-of-way required for CR 510 between US 1 and the Wabasso Causeway to reflect a right-of-way transition from the mainland to the bridge, thus minimizing the impact on residential areas; and
- when feasible, install streetlights that complement and enhance the surrounding architecture and landscaping.

**WABASSO CORRIDOR SPECIAL REGULATIONS FOR
NON-RESIDENTIAL DEVELOPMENT**

On a county-wide basis, Indian River County's existing land development regulations (LDRs) already address most of the development design issues on which the Wabasso Corridor Plan focuses. Thus, the existing LDRs set minimum, county-wide standards regarding allowable commercial uses (section 911.10), signs (section 956), screening of dumpsters (section 914.14(14)), building setbacks (section 911) and landscaping (section 926). These minimum standards, however, are not sufficient to preserve and promote Wabasso's special character and to meet the objectives for the Wabasso corridor as set forth in this plan.

Therefore, this plan contains special regulations for the Wabasso corridor that go beyond the county-wide minimum standards. These special regulations address certain design issues for new non-residential development and major redevelopment in the Wabasso Corridor Plan area. Accordingly, these regulations apply to all parcels within the corridor area. If any portion of a parcel is within the plan area, then the entire parcel is subject to these regulations. Within the plan area, both the county-wide regulations and these special regulations will apply. Where there is a difference between the county-wide LDRs and these special regulations, these regulations shall prevail.

NOTE: these special regulations are now codified and contained in LDR section 911.18.

SPECIAL REGULATIONS FOR MAJOR NEW DEVELOPMENT AND MAJOR REDEVELOPMENT

Future growth in Wabasso will mostly involve two types of major development. One type will be new major development projects occurring on vacant sites. The second type will be major

redevelopment projects entailing significant alteration and improvement of existing developed sites. Such new development or redevelopment will require compliance with special regulations designed to ensure that such projects will further the objectives of the Wabasso Corridor Plan.

In the Wabasso Corridor Plan area, the following special regulations shall apply to new non-residential development that requires major site plan approval.

- **Prohibited Uses:** Many commercial uses are necessary and appropriate within the county as a whole. However, certain uses, by their nature and appearance, are unaesthetic and inappropriate within the Wabasso Corridor Plan area. Generally, facilities that include outdoor display, storage and sales are difficult to shield with landscaping. Therefore, within the plan area, the following land uses shall be prohibited:
 - outdoor display of automobiles/motorized vehicles for sale or rental;
 - outdoor display of mobile homes for sale or rental;
 - outdoor display of boats for sale or rental;
 - drive-in theaters;
 - recycling centers;
 - transmission towers;
 - flea markets;
 - transient merchant uses; and
 - temporary sales events that require temporary use permits and are conducted outside of enclosed buildings.

Specifically, there shall be no outside display of merchandise on public sidewalks (within public rights-of-way or easements) or rights-of-way. Outside display of merchandise shall be allowed on private property; provided, however, that outside display of merchandise is allowed only on sidewalks abutting buildings occupied by the business displaying the merchandise. All other uses shall comply with applicable zoning district regulations.

- **Restricted Uses:** Certain uses and corresponding structures can be appropriate and compatible with the Wabasso Corridor Plan area IF properly oriented and screened. Therefore,

within the plan area, vehicle bays or stalls such as those associated with vehicle repair and car wash uses are allowed IF such bays or stalls are oriented and screened from view of US 1 and CR 510 by provision of a Type "B" buffer with four foot opaque feature.

Industrial and storage buildings located in the CH, IL, and IG zoning districts shall be exempted from foundation planting landscaping requirements and architectural/building requirements for building facades that do not abut residentially designated areas or front on public roads. However, industrial buildings shall satisfy the color requirements.

Electrical substations and similar uses that prohibit access by the public onto the site may be exempted from architectural/building requirements, if the exempted building(s) and equipment will be visually screened from adjacent properties and roadways.

• **Special Sign Regulations :**

- **Scope:** These special regulations consist of additional requirements above and beyond the county sign ordinance and shall supersede any less restrictive provisions found in the sign ordinance. All signage shall comply with the requirements of the sign ordinance except as modified by these special sign regulations.
- **Approval for change of sign design required:** Any exterior change to Wabasso Corridor signage which was originally required to comply with these special sign regulations shall require review and approval by the community development department. Such changes shall include, but not be limited to, changes of: sign area (square footage), sign copy area (square footage), height, shape, style, location, colors, materials, or method of illumination. Routine maintenance and replacement of materials which do not affect the approved design shall be exempt from this review and approval. Changes to signs not originally required to comply with these special sign regulations are addressed in the "nonconforming signs" section of these regulations.

• **Prohibited signs:**

Prohibited signs (this is in addition to sign ordinance section 956.12 prohibitions): The following are prohibited:

- ▶ Lights or illuminations that flash, move, rotate, scintillate, blink, flicker, or vary in intensity or color. Public signs permitted pursuant to sign ordinance section 956.11(2)(b) are excluded from this prohibition and are allowed.
- ▶ Electronic message boards and message centers, electronic adjustable alternation displays, or any sign that automatically displays words, numerals, and or characters in a programmed manner. Traffic regulatory and directional signs permitted pursuant to sign ordinance section 956.11(2)(b) are excluded from this prohibition and are allowed.
- ▶ Portable or trailer style changeable copy signs.
- ▶ Signs with the optical illusion of movement by means of a design that presents a pattern capable of giving the illusion of motion or changing of copy.
- ▶ Strings of light bulbs used on non-residential structures for commercial purposes, other than traditional holiday decorations at the appropriate time of the year.
- ▶ Signs that emit audible sound, odor, or visible matter, such as smoke or steam.
- ▶ Plastic or glass sign faces (including but not limited to: acrylic, Lexan^R, or Plexiglas^R). High density polyurethane and PVC are exempt from this prohibition. Portions of a sign which are changeable copy are exempt from this prohibition. When used in conjunction with cut-out or routered

metal cabinets, plastic used only for copy or logos is exempt from this prohibition. Plastic used for illuminated individual channel letters or logos is exempt from this prohibition. Although highly discouraged, a plastic sign face will be allowed only when all of the following requirements are met for the plastic portions of a sign:

Plastic shall be pan formed faced (embossed and/or de-bossed copy and logos are encouraged).

Regardless of the opaqueness of a sign, all plastic signage backgrounds shall be a dark color to reduce light transmission from signage background); white background shall not be allowed. All signage background colors shall be limited to those colors with a formula having a minimum black content of eleven (11) percent, and a maximum white content of forty-nine (49). Color formulas will be based on the Pantone Matching System^R.

All color must be applied on the "second surface" (inside face of plastic).

Nothing shall be applied to the "first surface" (outside face of plastic) (i.e.: paint, vinyl, etc.,)

- ▶ Neon and similar tube, fiber optic, and intense linear lighting systems, where the neon or lighting tube or fiber is visible.
- ▶ Plywood used for permanent signs.
- ▶ Any material used in such a manner for a permanent sign that results in a flat sign without dimension, having a semblance to a "plywood or temporary looking sign."
- ▶ Installation of an additional sign (or signs) that does not harmonize with the design or materials of the initial sign, such as:

Rear illuminated plastic faced sign with a "wood look" front illuminated sign.

Combination of signs with cabinets, faces or structure of awkwardly different materials or proportions.

Attachment or mounting of signs where mounting hardware is left exposed.

Signs with different color cabinets, frames, or structure.

- ▶ Individual styrofoam, plastic or wood letters or the like exceeding four (4) inches in height for use on any permanent monument, freestanding, roof, wall, or facade signs. This prohibition does not apply to illuminated individual metal channel letters or the plastic letter typically used for changeable copy signs.

- ***Signs that are encouraged:***

- ▶ Shaped and fashioned "wood look" multi level signs (i.e.: sand blasted or carved), and signs having durable sign cabinet material such as high density polyurethane and PVC, rather than actual wood or MDO.
- ▶ Internally illuminated aluminum cabinet with textured finish, and cut-out inset or push through acrylic letters. (Note that color may be applied to the "first surface" on push through acrylic letters for this type of sign.)
- ▶ Backlit reverse pan channel letters (opaque faced) mounted on sign that is harmonious with the project's architecture.
- ▶ Signage that relates to the building's style of architecture and materials.
- ▶ Thematic signage.

- ▶ Changeable copy signs that have a dark opaque background with translucent lettering.
- **Reduction in sign sizes and dimensions:** Modifications to Table 1 (freestanding signs) and Table 2 (wall signs), Schedule of Regulations for Permanent Signs Requiring Permits, sign ordinance.

Freestanding signs:

Maximum cumulative signage: Reduce to fifty (50) percent of what is allowed in Table 1.

Maximum signage on a single face: Reduce to fifty (50) percent of what is allowed in Table 1.

Maximum height: Reduce to thirty (30) percent of what is allowed in Table 1, but no less than six (6) feet and no greater than ten (10) feet.

For development involving sites of forty (40) acres or more, the ten (10) foot sign height requirement may be waived by the Board of County Commissioners if the development project applicant prepares and the Board of County Commissioners approves a sign package that reduces the total sign area otherwise allowed under the corridor plan and sign ordinance by ten (10) percent or more. Outparcels on larger sites shall comply with the six (6) feet to ten (10) foot height limitations specified above.

Required setbacks from property lines or right-of-way: One (1) foot subject to satisfaction of sight distance requirements.

Number of allowable signs per street frontage: No waivers shall be approved that would reduce the required minimum of two hundred (200) feet of separation between signs along the same street frontage found in Footnote #2 of Table 1 of the sign ordinance.

Wall/facade signs:

Maximum sign area allowed: Reduce to fifty (50) percent of what is allowed in Table 2.

Colors:

The following colors are **encouraged** for signage:

- ▶ Use of earth-tone colors and pastels.
- ▶ Darker backgrounds with light color sign copy.
- ▶ Use of colors that match or are compatible with the project's architecture.

The following colors are **prohibited** for signage:

- ▶ The use of shiny or bright metallic or mill finish colors (i.e.: gold, silver, bronze, chrome, aluminum, stainless steel, etc.). Colors such as medium or dark bronze are acceptable and encouraged. Polished or weathered true bronze, brass, or copper metal finishes are acceptable and encouraged. Precious metal colors are allowed on sand blasted or carved "wood look" style signs.
- ▶ The use of garish colors, such as fluorescent.
- ▶ The use of black for signage background. Changeable copy signage is excluded from this prohibition.

Multi-tenant spaces:

Applicants of proposed multi-tenant projects, such as shopping centers, out parcels, industrial complexes and parks, and office complexes and parks, shall submit a sign program for review and approval. This sign program shall identify the coordination and consistency of design, colors, materials, illumination, and locations of

signage. In a multi-tenant project where no established pattern, as described above exists, the owner of the multi-tenant project shall submit a sign program for approval prior to issuance of any new sign permits for a tenant space.

- ***Design criteria and additional restrictions:***

- ***Freestanding signage:***

All freestanding signs shall be of a wide-based monument style. Pole signs are discouraged, but may be permitted when the supporting structures are completely screened from view with landscaping or berm features. Said landscaping and/or berming shall cover and screen the entire area beneath the sign at time of certificate of occupancy (C.O.) issuance, and thereafter.

Any freestanding signs constructed from flat panel material, such as high density polyurethane, MDO, sheet metal, or the like, shall have a distance of no less than eight inches from face to face, and shall be enclosed on all sides to cover the internal frame.

- ***Freestanding changeable copy signs:***

Where, a freestanding changeable copy sign is allowed, no more than eighty (80) percent of the sign face area shall be comprised of changeable copy area.

- ***Wall/facade signage:***

The maximum vertical dimension of a facade or wall sign shall not exceed twenty-five (25) percent of the building height.

Awnings with lettering shall be considered wall signs. Where lettering is used on an awning, the area of lettering shall be included in the percentage limitation of a project's sign area.

Lettering, logos, and trim colors on canopy facia shall be considered a wall sign and shall be limited to thirty-

three (33) percent of the facia area of any one elevation. Internally illuminated signs shall not be placed on a canopy structure, and no sign shall be placed above the facia on a canopy structure.

Wall signs (facade signs) are prohibited on roofs with a slope less than 20:12 (rise:run) pitch. Wall signs mounted on a roof shall be enclosed on all sides to cover the internal frame and its connection to the roof. Also see IRC LDR's Section 956.12(1)(o).

Changeable copy wall signs for theaters:

Theaters may utilize up to eighty (80) percent of actual sign area for display of names of films, plays or other performances currently showing.

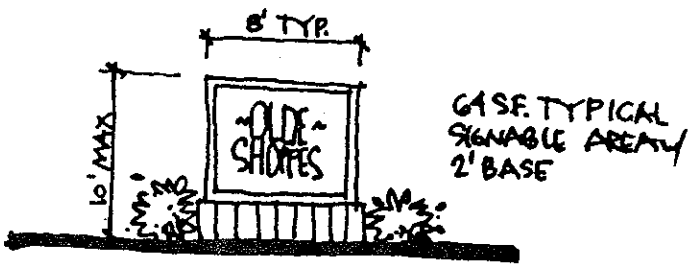
Illumination:

All external flood sign illumination shall be mounted at grade, directly in front of the sign area. Light source shall be completely shielded from oncoming motorist's view.

Nonconforming signs: It is the intent of these regulations to allow nonconforming signs to continue until they are no longer used or become hazardous, and to encourage conformance to these special sign regulations. A "compatible freestanding sign" shall be defined as any freestanding sign permitted prior to the adoption of these special regulations, and conforming to the Wabasso Corridor Plan maximum height requirements for a freestanding sign, and monument style for a freestanding sign.

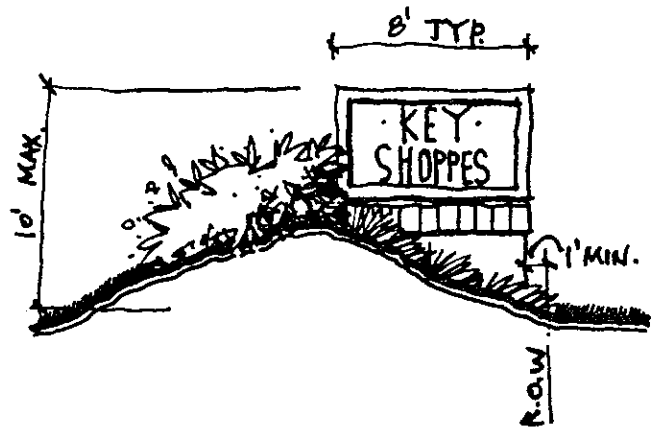
Nonconforming signs are subject to the following:

- ▶ Nonconforming signs or nonconforming sign structures on sites abandoned for twelve (12) or more consecutive months shall not be permitted for reuse.
- ▶ Except as otherwise provided herein, there may be a change of tenancy or ownership of a nonconforming



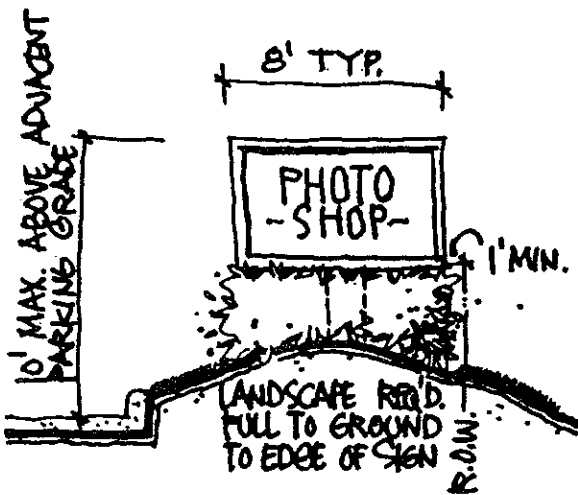
MONUMENT SIGN

MTS



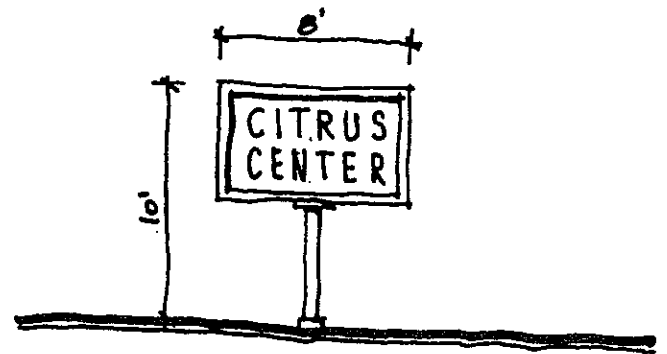
MONUMENT SIGN WITH SOLID BASE

MTS



PYLON SIGN ON CURB

MTS



PROHIBITED SIGN

MTS

sign, without the loss of nonconforming status, if the site is not abandoned for twelve (12) or more consecutive months.

- ▶ Colors of a nonconforming sign shall not be changed from those existing at the time of the adoption of this Code, unless new colors comply with the Wabasso Corridor special color requirements.
- ▶ A nonconforming sign shall not be enlarged or increased in any way from its lawful size at the time of the adoption of these special regulations.

Repairs and maintenance: Normal repairs and maintenance may be made: however, the cost of such repairs and/or maintenance made during any two-year period shall not exceed fifty (50) percent of the replacement cost of the sign at the end of the two-year period.

Reconstruction after catastrophe: If any nonconforming sign is damaged by fire, flood, explosion, collapse, wind, war, or other catastrophe, to such an extent that the cost of repair and reconstruction will exceed fifty (50) percent of the replacement cost at the time of damage, the nonconforming sign shall not be used or reconstructed except in full conformity with the provisions of these special regulations.

● **Architectural/building standards:**

Prohibited architectural styles: The following are prohibited:

- ▶ Corporate signature or commercial prototype architecture, unless such is consistent with these special corridor requirements. Examples of such prohibited architecture include flat roofed convenience stores, gas stations, and canopies for gas stations, car washes, and drive through facilities (see Figure B-1).
- ▶ Any kitsch architecture (such as a building that does not resemble a typical structure), including: structures or elements that resemble an exaggerated

plant, fish, edible food, or other such items such as giant oranges, ice cream cones, dinosaurs.

- ▶ Any architecture having a historical reference that is so different from current design philosophy that such reference is inconsistent and/or incompatible with surrounding structures. Examples of such include: igloos, domes or geodesic domes, Quonset style structures, teepees, western "false fronts," medieval castles, caves, and the like.

Architectural/building exemptions and special requirements:

Industrial and storage uses in the CH, IL, and IG Districts: Compliance shall be required only for those facades fronting on residentially designated areas or public or platted roads. However, industrial buildings shall satisfy the color requirements.

Electrical substations and similar uses: Electrical substations and similar uses that prohibit access by the public into the site may be exempted from all architectural/building requirements by the community development director if the exempted building(s) and equipment will be visually screened from adjacent properties and roadways.

General design criteria:

Buildings with facades fronting on more than one street shall have similar design considerations (e.g. exterior finish, roof treatment, building articulation, entrance features, and window placement) and consistent detailing on all street frontages.

General prohibitions and restrictions:

Flat, blank, unarticulated, or massive facades fronting on a roadway, exclusive access drive or residentially designated area are prohibited. Facades fronting such roads, drives, or areas shall be designed to incorporate architectural elements providing breaks in the planes of exterior walls and/or roofs to articulate the building and to lessen the appearance of excessive mass. Facades

should incorporate elements relating to human scale, and can be divided by use of: proportional expression of structure, openings, arcades, canopies, fenestration, changes in materials, cornice details, molding details, changes in the heights of different sections of the building, and the like (stepping or sloping of a parapet wall in conjunction with a low sloped roof is prohibited). Flat, blank, unarticulated, or massive facades will be permitted on the sides and rear of a building where "blank facade foundation plantings" are provided for such building facade faces (see foundation plantings section for requirements).

The following materials or systems are prohibited as a finish and/or exposed product: corrugated or ribbed metal panels, smooth finish concrete block (standard concrete masonry units), precast concrete tee systems, plywood or textured plywood. Plywood shall be allowed for soffit material.

Plastic or metal is prohibited as a finish material for walls or trim. Plastic is prohibited as a finish material for sloped roofs, visible roof structures, and facias. Although prohibited in general, certain metal and plastic construction products may be approved by the planning and zoning commission, upon a written request and product sample submitted by the applicant. The planning and zoning commission may approve use of the material if the following criteria are satisfied:

- The product shall appear authentic from the closest distance that it will be viewed by the general public.
- The product shall be substantial. Thin and flimsy imitations are unacceptable.
- The product shall hold up as well as the product it is imitating. That is, it must be fabricated in such a way that it will retain its original shape, appearance, and color, as well as the product it is imitating.

- The product's color shall resemble the color of the product it is imitating.

Any exposed masonry in a stack bond is prohibited.

Lighting structures or strip lighting that follows the form of the building, parts of the building, or building elements is prohibited.

Neon and similar tube and fiber optic lighting and similar linear lighting systems, where the neon or lighting tube or fiber is visible, is prohibited (this restriction includes site signage).

Backlit transparent or translucent architectural elements, backlit architectural elements, as well as illuminated or backlit awnings and roof mounted elements are prohibited. This does not prohibit the use of glass blocks. This does not prohibit the use of an illuminated sign attached to a building.

Facades that appear to be primarily awnings are prohibited. An awning shall not run continuously for more than thirty (30) percent of the length of any single facade. Gaps between awning segments shall be at least twenty-four (24) inches wide. Placement of awning segments shall relate to building features (e.g. doorways and windows), where possible. Awnings shall not exceed twenty-five (25) percent of the area of any single facade.

Drive-up windows shall not be located on a building facade that faces a residential area or a roadway unless architecturally integrated into the building and screened by landscaping equivalent to the material in a local road buffer that runs the length of the drive-through lane and its speakers shall be oriented so as not to project sound toward residential areas.

Accessory structures, including sheds, out buildings, dumpster enclosures, and screening structures, shall match the style, finish, and color of the site's main

building. Metal utility sheds and temporary car canopies are prohibited.

- **Roofs:** Use of sloped roofs on buildings will preserve Wabasso's existing and desired character and will be more aesthetically compatible with existing buildings in Wabasso than buildings with flat roofs. Therefore, all buildings and accessory structures within the plan area shall have sloped roofs (slope pitch at least 4:12) visible from every direction, unless a visible flat roof, parapet roof, or other such roof design is determined by the community development director or his designee to be an integral feature of a recognized architectural style. All buildings and accessory structures may have flat roof systems where flat roof areas are not visible at 6' above grade from all directions.

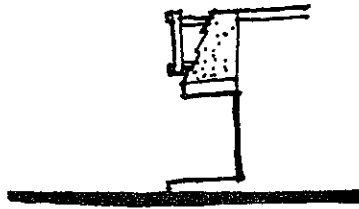
(Related Graphics on Next Page)

Sloped roofs, including mansards, shall have a minimum vertical rise of six feet (not including fascia). Where flat roofs are allowed by the community development director or his designee, buildings and accessory structures may have flat roof systems where flat roof areas are not visible at six (6) feet above grade from all directions.

The ridge or plane of a roof (or visible roof structure) that runs parallel (or slightly parallel) with a roadway shall not run continuous for more than one hundred (100) feet without offsetting or jogging (vertically or horizontally) the roof ridge or plane a minimum of sixteen (16) inches (see Figure B-2). Low slope roofs and parapet walls allowed by the community development director or his designee are excluded from this requirement.

Roofing materials are prohibited for use as a finish material on parapets or any surface with a slope greater than 30:12 (rise:run), up to and including vertical surfaces. This pertains only to those surfaces visible from adjacent property, exclusive access drives or roadways.

"F-2: ROOFS"

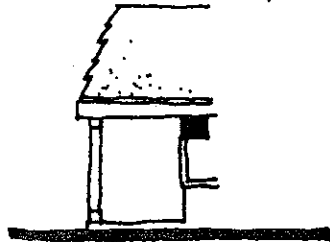


SIDE VIEW

"STUCK ON" MANSARD ROOF (prohibited)

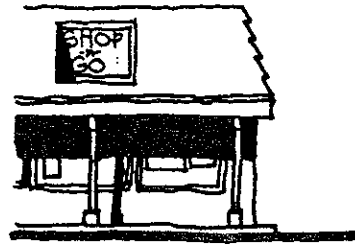


FRONT VIEW

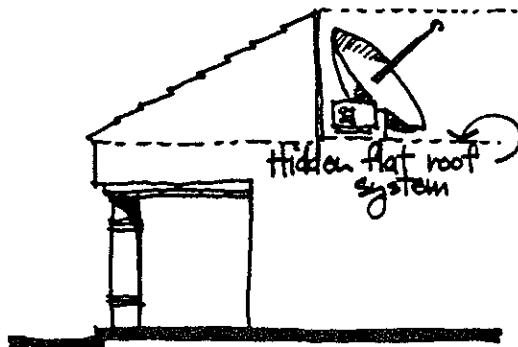


SIDE VIEW

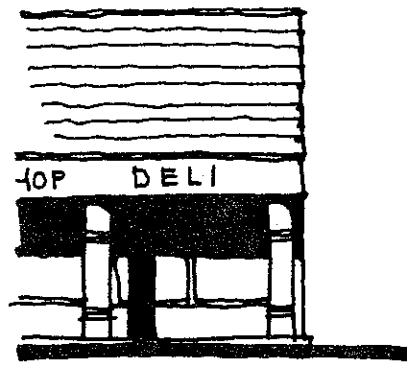
"TRUE" MANSARD ROOF (allowed)



FRONT VIEW



SIDE VIEW



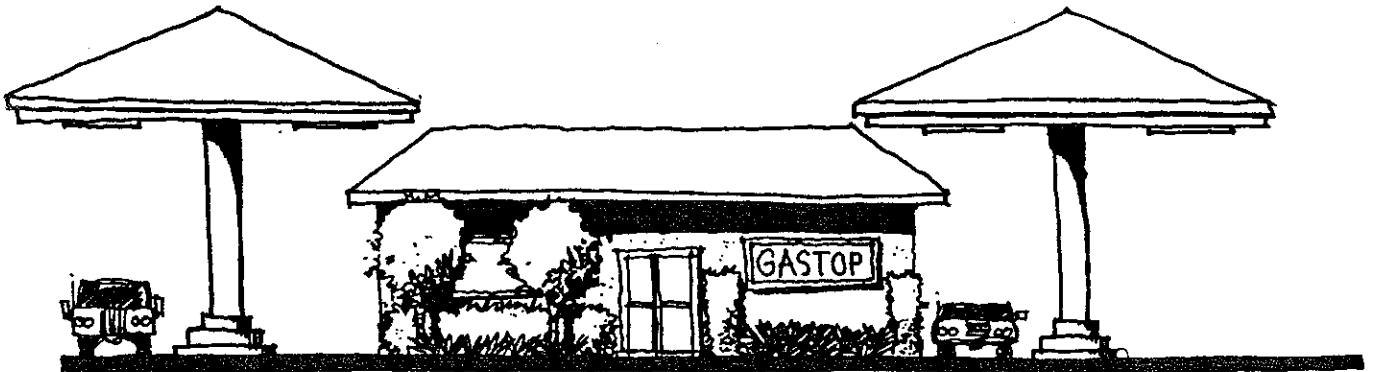
FRONT VIEW

HIDDEN FLAT ROOF (allowed)

"F-3: ROOFS"



"NO" (prohibited)



"YES" (allowed)

- **Colors:**

Colors and building graphics

The following building graphics are prohibited: polka dots, circles, vertical stripes, diagonal stripes or lines, plaids, animals, and symbols such as lightning bolts. However, legally registered trademarks which directly relate to the building occupant (not trademarks of products or services sold or displayed) are allowed, subject to applicable sign and color regulations.

Color standards: All buildings and accessory structures within the Wabasso Corridor shall be limited to the following colors:

Base building colors: Base building colors relate to wall and parapet wall areas and shall be limited to the colors listed in the Wabasso Corridor Master Color List. These colors consist of white and light neutral colors in the warm range.

Secondary building colors: Secondary building colors relate to larger trim areas and shall be limited to the colors listed in the Wabasso Corridor Master Color List. Secondary building colors shall not exceed thirty (30) percent of the surface area of any one building facade elevation. These colors consist of a mid-range intensity of the base building colors and complementary colors, and include all base building colors.

Trim colors: Trim colors are used for accent of smaller trim areas, are the brightest group of colors allowed, and include all base building and secondary building colors. Use of metallic colors (i.e.: gold, silver, bronze, chrome, etc . . .) and use of garish colors, such as fluorescent colors (e.g. hot pink, shocking yellow), are prohibited. Trim colors shall not exceed ten (10) percent of the surface area of any one building facade elevation. Where trim colors are used in a building facade sign, the trim color area of the facade sign shall be included in the percentage limitation on the trim color surface area.

Roof colors: (requirements for roofs that are visible from a roadway and/or residentially designated area) Metal roof colors shall be limited to the colors listed in the Wabasso Corridor Master Color List's "Metal Roof Colors." These colors consist of natural mill finish, white, light neutral colors in the warm range,

blue, and a limited number of earth-tone colors. Mixing or alternating colors of metal panels is prohibited. For non-metal roofs, other than natural variations in color or color blends within a tile, the mixing or alternating of roof color in the same roof material is prohibited. Colors and color blends shall not be contrary to the intent of this code. Color for roofing which is glazed, slurry coated, or artificially colored on the surface by any other means shall be limited to the same colors as approved for metal roofs.

Natural finish materials: The color requirements listed above shall not apply to the colors of true natural finish materials such as brick, stone, terra cotta, concrete roof tiles, slate, integrally colored concrete masonry units, copper, and wood. Colors commonly found in natural materials are acceptable, unless such material has been artificially colored in a manner which would be contrary to the intent of these requirements. Black, gray, blue, or extremely dark colors for brick, concrete masonry units, roofing, wood or stone is prohibited. (This provision shall not prohibit the use of colors for natural finish roofing materials that match those colors approved for metal roofs.)

Awning colors: Awning colors may include base building colors and/or secondary building colors and/or trim colors. However, secondary building colors and trim color area used for awnings shall be included in the percentage limitation on the secondary building color and trim color surface area of a facade.

The Wabasso Corridor Master Color List and the approved color board are the same as for the SR 60 Corridor Plan, and shall be maintained by and made available by planning staff. The list can be mailed or faxed upon request.

- **Special screening and lighting:** Equipment customarily found in association with buildings can greatly detract from the aesthetic appearance of buildings unless such equipment is properly screened from view on all sides or integrated into the overall appearance of the building by architectural design and features. Therefore, within the plan area, mechanical equipment (ground, building, and roof mounted), including air conditioning units, pumps, meters, walk-in coolers, and similar equipment shall be visually screened from surrounding

properties and roadways using architectural features, fencing, walls, or landscaping.

In addition to required landscaping, all loading/unloading dock areas located adjacent to residentially designated areas and/or roadways shall be provided with a solid wall at least 8' in height above the loading area grade, to buffer adjacent roadways and residentially designated sites from noises and sights associated with docks.

Man-made opaque screens which are visible from any public or private right-of-way or street, or any residentially designated area, shall be constructed of a material which is architecturally similar in design, color and finish to the principal structure.

All telephones, vending machines, or any facility dispensing merchandise or a service on private property shall be confined to a space built into the building or buildings, or enclosed in a separate structure compatible with the main building's architecture. These areas are to be designed with the safety of the user in mind. Public phones and ATMs should have twenty-four-hour access.

No advertising will be allowed on any exposed amenity or facility such as benches or trash containers.

Screening of chain link fencing. Where chain link or similar fencing is allowed to be used (e.g. around stormwater ponds), such fencing shall be green or black and shall be located and landscaped so as to visually screen the fencing from public view.

Lighting

The use of thematic and decorative site lighting is encouraged. Low lights of a modest scale can be used along with feature lighting that emphasizes plants, trees, entrances, and exits. Light bollards are encouraged along pedestrian paths. The color of the light sources (lamp) should be consistent throughout the project. Color of site lighting luminaries, poles, and the like shall be limited to dark

bronze, black, or dark green (decorative fixtures attached to buildings are exempt from fixture color requirement).

Lighting is not to be used as a form of advertising or in a manner that draws considerably more attention to the building or grounds at night than in the day. Site lighting shall be designed to direct light into the property. It is to avoid any annoyance to the neighbors from brightness or glare.

Roadway style luminaries (fixtures) such as cobra heads, Nema heads, and the like are prohibited. Wall pack and flood light luminaries are prohibited where the light source would be visible from a roadway, parking area and/or residentially designated area. High intensity discharge (e.g. high pressure sodium, metal halide, mercury vapor, tungsten halogen) lighting fixtures mounted on buildings and poles higher than eighteen (18) feet above parking lot grade, and under canopies, shall be directed perpendicular to the ground. Other than decorative and low level/low height lighting, no light source or lens shall project above or below a fixture box, shield, or canopy.

- **US 1 and CR 510 Landscape Buffer:** Landscape buffers help to separate uses and provide an open and natural appearance. Such buffers can also hide what is unaesthetic and complement what is aesthetic.

To enhance the appearance of the corridor from US 1 and CR 510 and from adjacent sidewalks and bikeways, to maintain an open and natural appearance to the corridor, and to retain and promote property values within the corridor, a special landscape feature along the US 1 and CR 510 frontages of new development sites is necessary. Therefore, within the plan area the following landscape requirements shall apply.

- Landscape Buffer Along US 1 and CR 510

Increased canopy tree size. All canopy trees required under normal landscaping and buffering requirements and special Wabasso corridor plan requirements for projects within the corridor shall have a minimum height of twelve (12) feet and minimum spread of six (6) feet at time of planting. Palm tree clusters may be used as canopy

trees as specified in the landscape ordinance. However, such palm trees shall have a minimum clear trunk of twelve (12) feet.

The county-wide landscaping requirements of LDR Chapter 926 shall apply except as noted in this plan. The following landscape buffer shall be provided along the entire length of a site's CR 510 and US 1 frontages, except for approved driveways:

<u>Buffer Width</u>	<u>Minimum Planting/Berm Requirements per 100'</u>
20' or more	4 canopy trees 5 understory trees Continuous hedge*: 1½'-2½' high at planting Berm: 1½'-3' high*
15'	4.5 canopy trees 5.5 understory trees Continuous hedge*: 1½'-3' high at planting Berm: 1'-2½' high*
10'	5 canopy trees 6 understory trees Continuous hedge*: 2½'-3' high at planting Berm: 1'-1½' high*

***NOTE:** The intent of the hedge and berm combination is to provide a visual screen 4' high above the grade of the project site parking area. Therefore, at the time of a certificate of occupancy (CO) for the project site, the combination of berming and hedging shall provide a 4' visual screen. Undulations in the berm and corresponding hedge height are encouraged.

Hedge shrubs shall be planted no further apart than 24" on center, in a serpentine pattern along the length of the buffer strip. Berms shall have a slope no steeper than 3 horizontal to 1 vertical, and shall be continuous along the length of the buffer strip, except where berm modifications may be necessary for tree

preservation as determined by the community development director or his designee.

To provide a less formal appearance, clustering trees along the buffer strip is encouraged and uniform spacing of trees is discouraged, except where used to emphasize a particular planting theme or development style.

● **Landscape Buffer Along Other Streets in the Plan Area:** LDR Chapter 926 requires that where parking lots abut road rights-of-way, there must be a landscaped strip that is at least 10' wide that must contain 1 canopy tree for every 30 lineal feet and a continuous hedge. In addition to these standard requirements, within the corridor plan area such landscape strips must also contain 2 understory trees for every 30 lineal feet.

● **Landscape Buffer Along Commercial/Residential Border:** LDR section 911.10(8) requires a buffer and an opaque feature where commercial development in a commercial zoning district abuts a residential district or use. Depending on the type of development and the zoning districts involved, the buffer may be type "B", "C", or "D", and the required opaque feature may range in minimum height from 3' to 6'. Within the corridor plan area, where these buffers are required, 2 additional understory trees per 30 lineal feet of required buffer strip shall be provided.

● **Foundation Plantings:** Foundation plantings shall be required as stated below for buildings in commercial and industrial areas and for businesses allowed in residential areas. However, for industrial and storage buildings located in the CH, IL, and IG zoning districts, foundation planting strips shall be exempted for sides of buildings not fronting on a residentially designated area, or public or platted road.

Along the front, sides and rear of buildings, the following foundation planting landscape strips shall be provided in accordance with the building height:

Building Height	Foundation Planting Strip Depth ¹
Up to 12' high	5' depth
12' to 25' high	10' depth
Over 25' high	15' depth

¹A distance measured perpendicular to the building, from the foundation outward

Within such foundation planting landscape strips, the following landscaping shall be provided:

Forty (40) percent of the foundation perimeter (excluding entranceways and overhead doors) along all building faces shall be landscaped, as follows:

*For buildings up to 12' in height	
Minimum planting area depth:	5'
Minimum plant material required:	*1 palm tree or appropriate canopy tree for every 10 lineal feet of planting strip (clustered)
	*1 understory tree for every 20 lineal feet of required planting strip
	*3 shrubs for every 10 square feet of required planting area

	*Ground cover, flowering plants or sod in the remaining planting area
*For buildings of 12' to 25' in height	
Minimum planting area depth:	10'
Minimum planting material required:	*1 canopy tree for every 10 lineal feet of required planting strip (3 palms with a minimum height of 12' each may be substituted for each canopy tree)
	*1 understory tree for every 20 lineal feet of required planting strip
	*3 shrubs for every 10 square feet of required planting area
	*Ground cover, flowering plants or sod in the remaining planting area
*For buildings over 25' in height	
Minimum planting area depth:	15'

<p>Minimum plant material required:</p>	<p>*1 canopy tree for every 7 lineal feet of required planting strip (3 palms with a minimum height of 16' each may be substituted for each canopy tree)</p>
	<p>*1 understory tree for every 10 lineal feet of required planting area</p>
	<p>*3 shrubs for every 10 square feet of required planting area</p>
	<p>*Ground cover, flowering plants or sod in the remaining planting area</p>

The following modifications are allowed upon approval from the community development director or his designee:

Foundation planting strips may be located away from buildings to avoid conflicts with architectural features (e.g., roof overhangs), driveways, and vehicular areas serving drive-up windows.

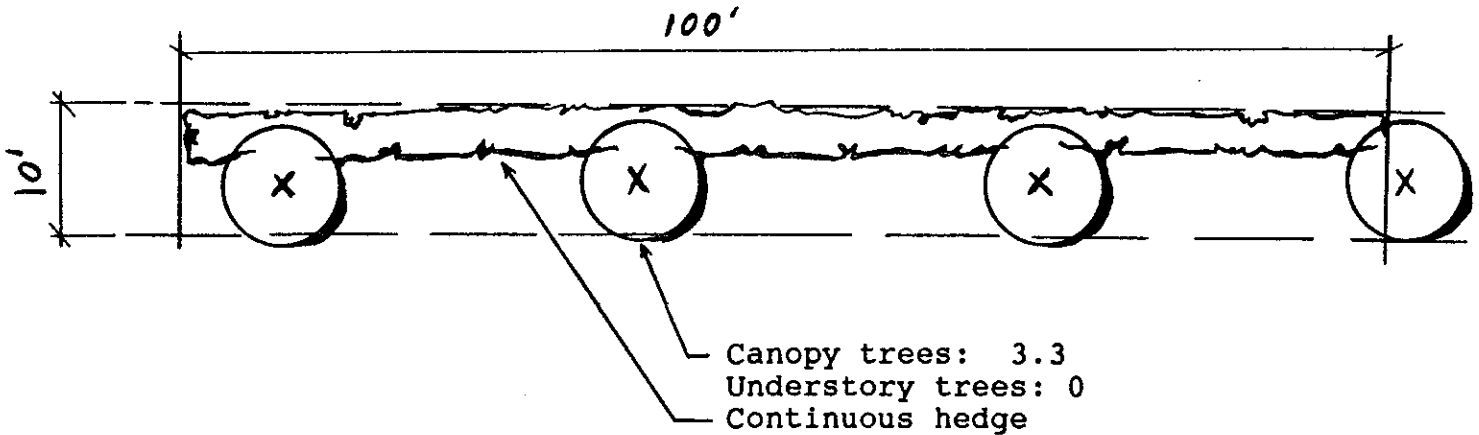
The depth of foundation planting strips may be modified if the overall minimum area covered by the foundation plantings proposed meets or exceeds the area encompassed by a typical layout.

Increased foundation plantings for "blank facade" building faces. As referenced in the architectural/building standards section of this plan, "blank facade" building faces that are unarticulated are allowed if foundation plantings are provided as specified above with a one hundred (100) percent increase (doubling) in required plant material quantities (as specified above).

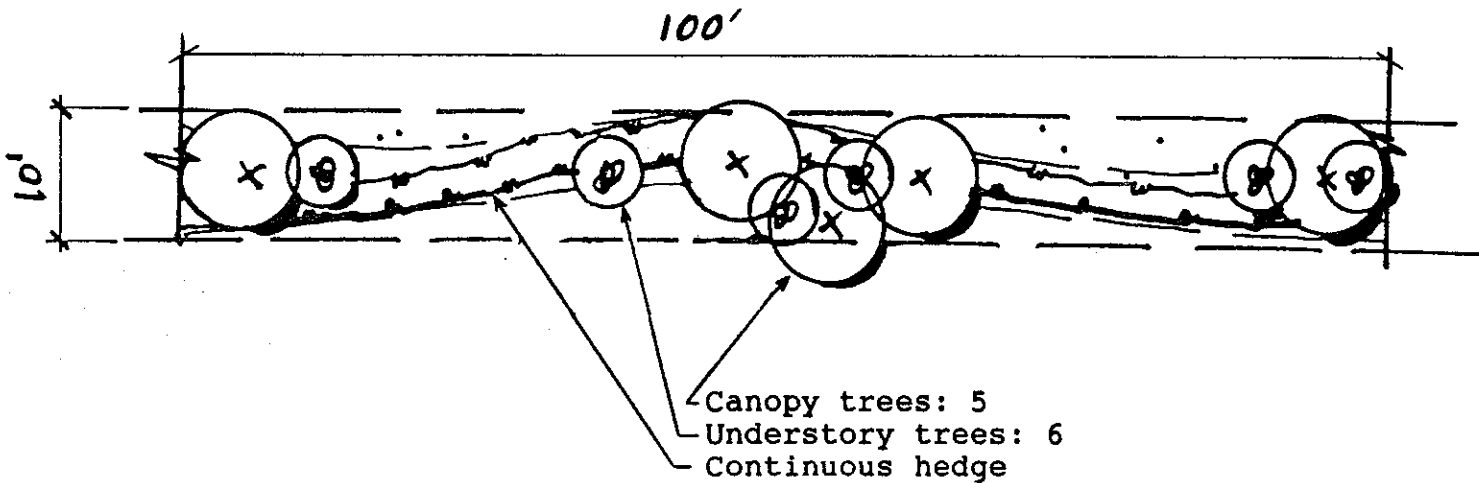
Figure A

US1 AND CR 510 BUFFER STRIP

Existing Requirements



Plan Requirements



Differences Per 100':

Canopy trees:	+1.7/\$300 (@ \$175 each)
Understory trees:	+6.0/\$450 (@ \$75 each)
Total cost difference:	\$750 per 100' of required US 1 or CR 510 buffer strip

NOTE: 20' wide buffer requires +0.7 canopy and +5 understory trees for a difference of \$500 per 100' of required US 1 or CR 510 buffer strip

Figure B

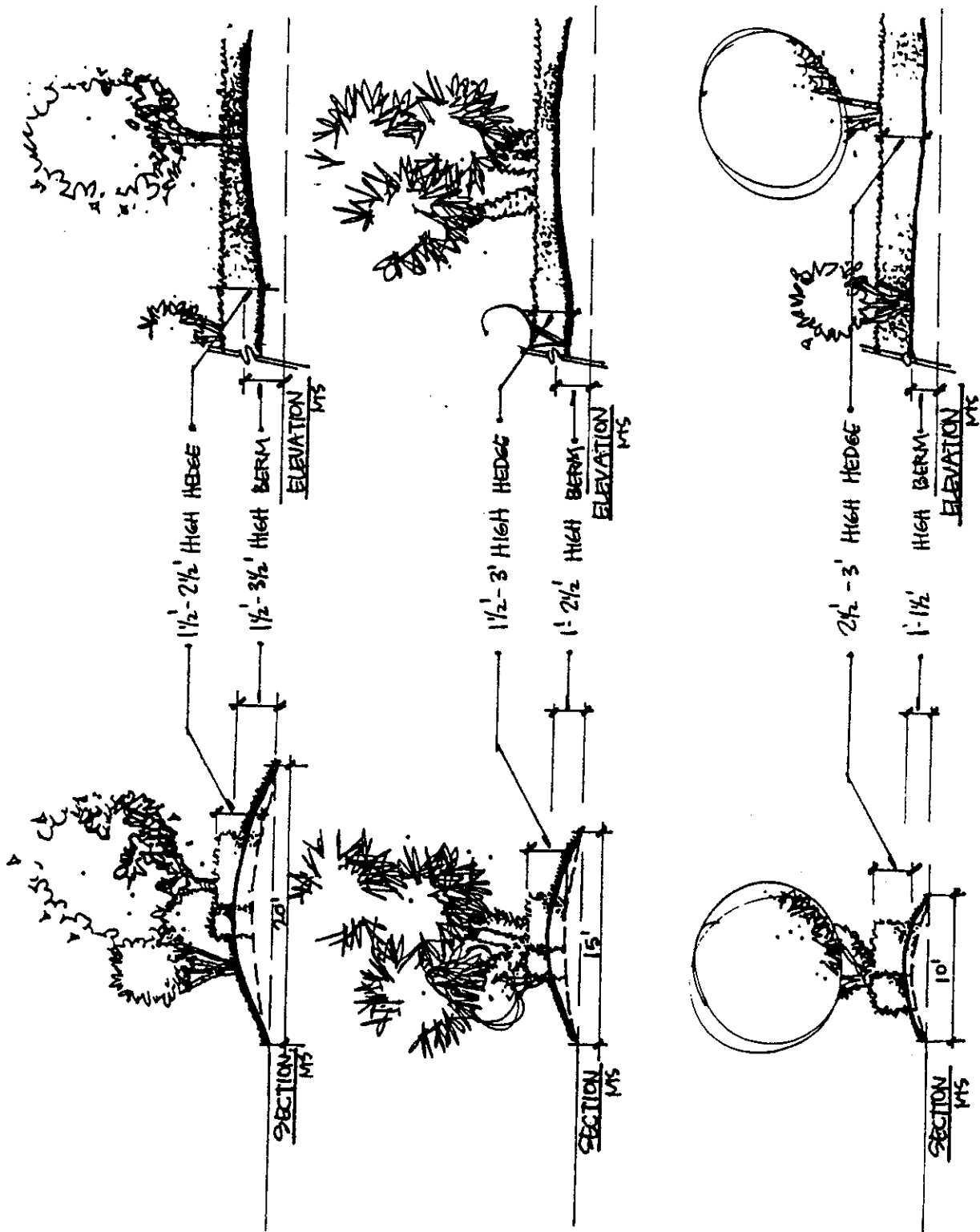
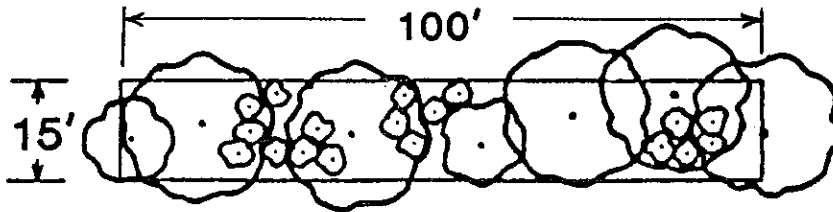


Figure C

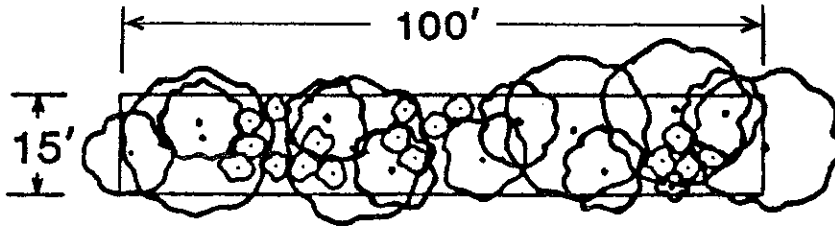
COMMERCIAL/RESIDENTIAL BUFFER

Existing Regulation
[Example: 15' wide Type "C" Buffer]



4.5 - Canopy
1.8 - Understory
18.0 - Shrubs

Recommended Requirements

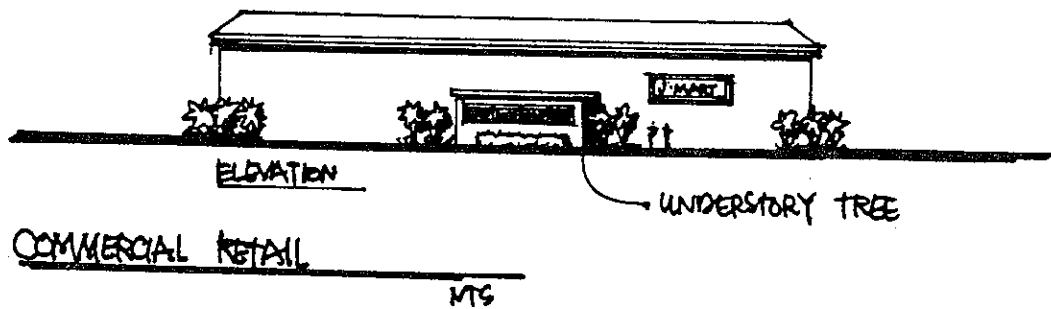
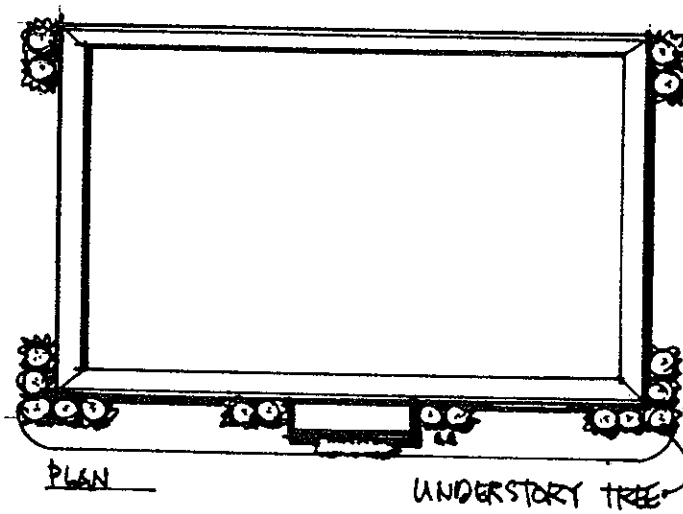


4.5 - Canopy
7.8 - Understory
18.0 - Shrubs

Cost Difference: In the above example strip of 100', add in 6 understory trees at \$75.00/each = \$450.00 additional cost

"F-6: FOUNDATION PLANTINGS"

Figure D



LANDSCAPE PLANTING THEME

All property adjacent to US 1 and CR 510 within the corridor has been placed into one of four (4) landscaping theme categories. Within each theme category, required landscape plantings shall be limited to certain species, as specified below, with the exception that sod, annuals, or hardscape may be used in groundcover areas.

FORMAL PALM THEME - CR 510 between Massey Road and the eastern foot of the bridge

Street Tree: (Canopy, Min. 8' Clear Trunk)

Washingtonia robusta	Washington Palm
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Supporting Shade Tree: (Canopy, Min. 12'-14' Height. x 6' Spread)

Quercus virginiana	Live Oak
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Accent Trees: (Understory, Min. 8' Height. x 4'-6' Spread)

Ligustrum lucidum	Tree Ligustrum
Nerium oleander	Oleander Tree
Myrica cerifera	Wax Myrtle

Shrubs: (Min. 24" Height)

Nerium oleander	Oleander
Myrica cerifera	Wax Myrtle
Ligustrum lucidum	Ligustrum
Viburnum suspensum	Sandankwa Viburnum
Coccoloba uvifera	Seagrape
Livistona chinensis	Chinese Fan Palm

Groundcover: (Min. 10"-12" Height)

Raphiolepis indica	Indian Hawthorn
Ilex vomitoria	Yaupon Holly
Liriope muscari "Evergreen Giant"	Evergreen Giant Liriope
Pittosporum tobira "variegata"	Variegated Pittosporum
Pittosporum tobira	Green Pittosporum
Ixora "Nora Grant"	Nora Grant Ixora
Carissa grandiflora	Natal Plum

INFORMAL PALM THEME - SR A1A/CR 510 intersection west to the eastern foot of the bridge

Street Trees: (Canopy, 10'-16' Clear Trunk)

Washingtonia robusta	Washington Palm
Sabal palmetto	Cabbage Palm

Supporting Shade Tree: (Canopy, 12'-14' Height)

Quercus virginiana	Live Oak
--------------------	----------

Accent Trees: (Understory, Min. 8' Height x 4'-6' Spread)

Nerium oleander	Oleander Tree
Myrica cerifera	Wax Myrtle
Ligustrum lucidum	Tree Ligustrum

Shrubs: (Min. 24" Height)

Scaevola	Inkberry
Nerium oleander	Oleander
Elaeagnus	Silverthorn
Myrica cerifera	Wax Myrtle
Ligustrum lucidum	Ligustrum
Viburnum suspensum	Sandankwa Viburnum
Tripsacum dactyloides	Fakahatchee Grass
Coccoloba uvifera	Seagrape

Groundcover: (Min. 10"-12" Height)

Lantana	Lantana
Liriope muscari "Evergreen Giant"	Evergreen Giant Liriope
Carissa grandiflora	Natal Plum
Pittosporum tobira "variegata"	Variegated Pittosporum
Ilex vomitoria	Yaupon Holly
Raphiolepis indica	Indian Hawthorn

FORMAL OAK THEME - US 1 from 84th Street north to Bridge Boulevard (87th Street); CR 510 between FEC Railroad and Massey Road

Street Trees: (Canopy, Min.14' Height x 6'-7' Spread 2½"-3" Cal.)

<i>Quercus laurifolia</i>	Laurel Oak
<i>Quercus virginiana</i>	Live Oak

Accent Trees: (Understory, Min. 8' Height x 4'-6' Spread)

<i>Lagerstroemia indica</i>	Crape Myrtle
<i>Myrica cerifera</i>	Wax Myrtle
<i>Ligustrum lucidum</i>	Tree Ligustrum
<i>Ilex attenuata</i> "East Palatka"	East Palatka Holly
<i>Nerium oleander</i>	Oleander Tree

Shrubs: (Min. 24" Height)

<i>Viburnum suspensum</i>	Sandankwa Viburnum
<i>Ligustrum lucidum</i>	Ligustrum
<i>Viburnum odoratissimum</i>	Sweet Viburnum
<i>Myrica cerifera</i>	Wax Myrtle

Groundcover: (Min. 10"-12" Height)

<i>Raphiolepis indica</i>	Indian Hawthorn
<i>Ilex vomitoria</i>	Yaupon Holly
<i>Liriope muscari</i> "Evergreen Giant"	Evergreen Giant Liriope
<i>Hemerocallis</i> spp.	Daylily
<i>Juniper chinensis</i> "parsonii"	Parson's Juniper
<i>Hymenocallis latifolia</i>	Spider Lily
<i>Ixora</i> "Nora Grant"	<i>Ixora Nora Grant</i>

INFORMAL NATIVE THEME - CR 510 west of FEC Railroad; US 1 (81st Street to 87th Street); US 1 (87th Street to 95th Street)

Street Trees: (Canopy, 10'-16' Height)

Pinus elliottii	Slash Pine
Quercus virginiana	Live Oak
Quercus laurifolia	Laurel Oak
Sabal palmetto	Cabbage Palm

Accent Trees: (Understory, Min. 8' Height x 4'-6' Spread)

Ilex cassine	Dahoon Holly
Myrica cerifera	Wax Myrtle
Ligustrum lucidum	Tree Ligustrum
Lagerstroemia indica	Crape Myrtle
Ilex attenuata "East Palatka"	East Palatka Holly

Shrubs: (Min. 24" Height)

Crinum spp.	Crinum Lily
Elaeagnus pungens	Silverthorn
Syzygium paniculata "myrtifolia"	Eugenia myrtifolia
Livistona chinensis	Chinese Fan Palm
Myrica cerifera	Wax Myrtle
Philodendron selloum	Philodendron
Viburnum odoratissimum	Sweet Viburnum
Viburnum suspensum	Sandankwa Viburnum

Groundcover: (Min. 10"-12" Height)

Nephrolepis	Swordfern
Liriope muscari "Evergreen Giant"	Evergreen Giant Liriope
Hymenocallis Latifolia	Spider Lily
Pittosporum tobira "Variegata"	Variegated Pittosporum
Lantana	Lantana
Juniperus chinensis "parsonii"	Parson's juniper
Dietes vegeta	African Iris